

Planning Commission Hearing Staff Report

McDonald's Design Review for Rebuild
Conditional Building and Site Design Review
(Petition 430-08-01)
1533 South State Street
Hearing date: June 11, 2008



Planning and Zoning Division
Department of Community &
Economic Development

Applicant:

McDonald's Corporation
E. Farley Eskelson (Dominion
Engineering)

Staff:

Casey Stewart 535-6260
casey.stewart@slcgov.com

Tax ID:

16-18-153-008, 16-18-153-003, 16-18-
153-010

Current Zone:

CC (Corridor Commercial)
SSSC (South State Street Corridor
Overlay)

Master Plan Designation:

Central Community:
Community Commercial

Council District:

District 5 – Jill Remington Love

Lot size: 0.82 acres

Current Use:

Restaurant with drive-through

Applicable Land Use Regulations:

- Chapter 21A.26.050 CC Zoning District
- Chapter 21A.34.090 SSSC overlay district
- Chapter 21A.59 Conditional Building and Site Design Review

Attachments:

- A. Applicant's Project Description
- B. Site and building drawings
- C. Site Photographs
- D. Department comments
- E. Public Comments
- F. PC Subcommittee notes

REQUEST

This is a request by McDonald's Corporation for Conditional Building and Site Design Review approval by the Planning Commission for rebuilding of McDonald's restaurant located at 1533 South State Street. Conditional Building and Site Design Review is required because the proposed building location, vehicle parking areas and associated landscaping, and proposed amount of first floor glass do not comply with the Ordinance.

PUBLIC NOTICE

On May 27, 2008 a notice of public hearing was mailed to all property owners abutting and contiguous to the subject property and on May 30, 2008 the site was posted with a notice of public hearing sign. Both the mailed and posted notice comply with noticing requirements of the City Ordinance. Community Council Chairs, Business Groups and other interested parties were notified through the Planning Division's listserv. The Planning Commission agenda was posted on the Planning Division's web page.

PUBLIC COMMENTS

The subject property is located within 600 feet of two different community councils, Liberty-Wells and People's Freeway. In order to more efficiently gather comments from the public, a public open house was held on April 17, 2008. One person, a representative from Salt Lake Community College, attended the open house and offered suggestions for allowing easier pedestrian flow between the College and the restaurant. The applicant agreed to remove the existing block wall and fencing that separate the two properties and install a pedestrian path between the two buildings.

STAFF RECOMMENDATION:

Staff finds that, as proposed, the Conditional Building and Site Design Review application by McDonald's Corporation (petition #430-08-01) does not satisfy all of the standards for approval (21A.59.060) and therefore cannot recommend approval. Staff recommends instead that the project be approved subject to only those design modifications specified below and the following conditions:

Allowed modifications:

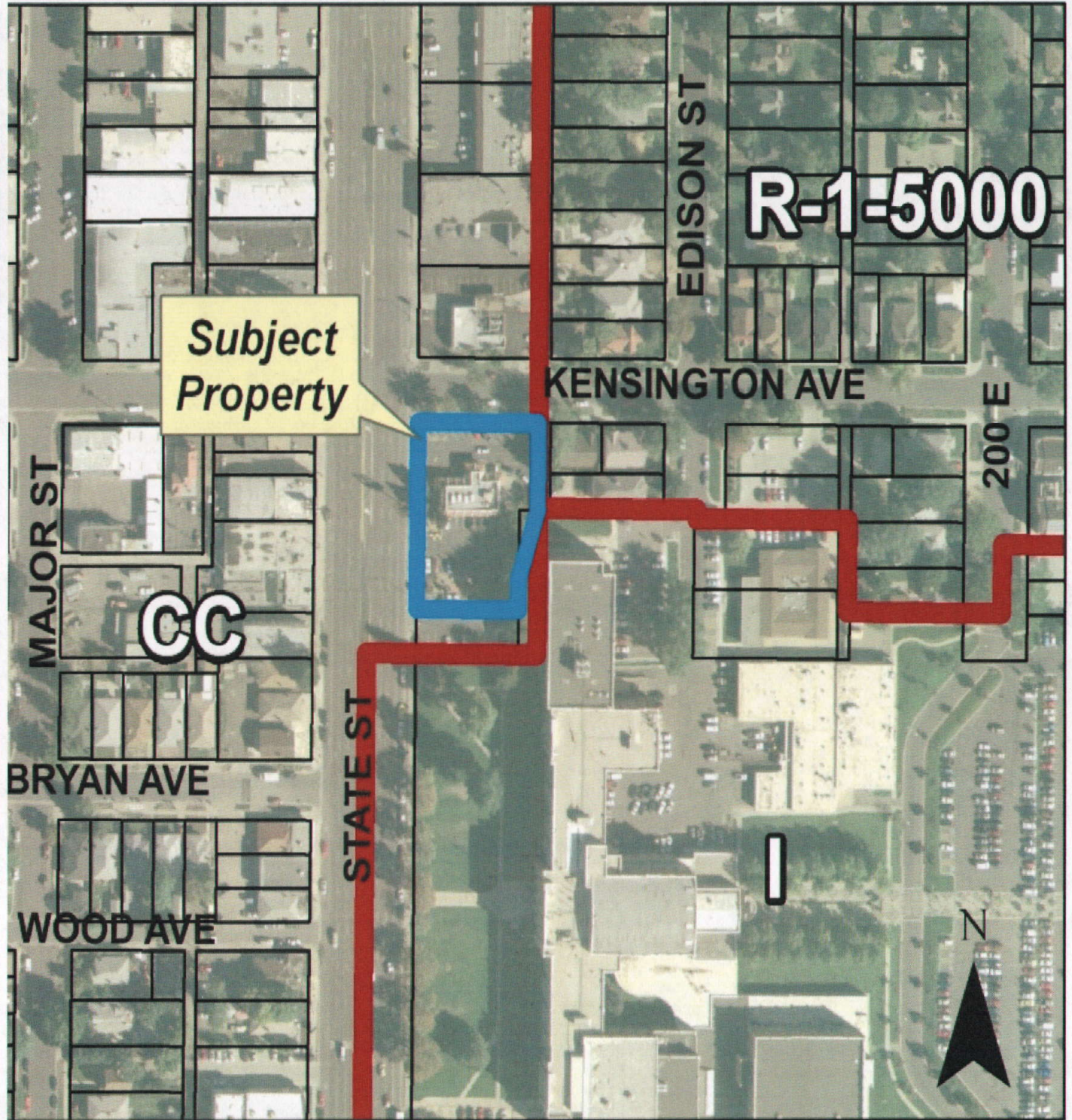
1. Reduction from 7 feet down to 5 feet for width of required landscape buffer along north edge of parking lot along Kensington Ave.
2. Reduction from 15 feet to 5 feet for required parking lot setback along Kensington Ave.
3. Reduction from 40% to 7% for required glass content along north façade.
4. Additional parking stalls, beyond the number required based on the size of the building, will not be required as a result of the proposed outdoor dining.

Conditions of approval:

1. Combine the three existing lots into one through an appropriate legal, city-approved method.
2. Replace the two existing evergreen trees along State Street with two shade trees.
3. Compliance with the departmental comments as outlined in this staff report.
4. Approval of the design shall be void unless a building permit has been issued or use of the land has commenced within twelve (12) months from the date of approval. Upon request, revalidation of the site plan may be granted for an additional twelve (12) months by the Planning Commission if all factors of the original approved design are the same.

VICINITY MAP

1533 S. State Street



Overview

The project site is located at 1533 S. State Street at the southeast corner of State Street and Kensington Avenue, in the Corridor Commercial district (CC) and the South State Street Corridor overlay district (SSSC). The applicant proposes to demolish the existing McDonald's restaurant and build a new McDonald's restaurant in the same spot. The new restaurant will have a slightly larger footprint with an outdoor seating area and the parking and traffic circulation will be revised. The "drive-through" is to be located on the north side of the building and is accessible, via a circumscribing drive aisle, from any area of the site. The proposal is being processed through the Conditional Building and Site Design Review because the applicant seeks modification to the maximum front setback, parking lot and associated landscaping, and building glass content requirements of the SSSC overlay district.

Existing Conditions

The uses surrounding the site include the Salt Lake Community College to the south and east, restaurants to the north and northwest, a pawn shop and other small businesses to the west.

The project site currently consists of three separate parcels. To satisfy the requirement that all parking for the use be located on site, and to avoid the requirement to install a 7-foot landscaping buffer around parking lots on individual parcels, the parcels should be consolidated into one. Allowing the three parcels to remain would unnecessarily break up the parking lot into the separate lots without circulation between them and would require unnecessary off-site parking agreements.

The existing parking lot has 51 stalls. Most of the restaurant's business results from drive-through customers. The drive-through is located on the north side of the building. A combination fence/concrete wall borders the east and south property lines, separating the restaurant from the Salt Lake Community College campus. Vehicle access is provided through two curb cuts from State Street for north bound traffic and from one curb cut from Kensington Avenue for east and west-bound traffic. South bound traffic must turn at Kensington to access the north parking area. Vehicle parking currently extends into the front yard (State Street) and corner side yard (Kensington).

Discussion

The following discussion clarifies the specific reasons for Conditional Building and Site Design Review of this project and staff's consideration of each reason. The requirements discussed come from the SSSC overlay district, the landscaping ordinance for parking lots, and off street parking requirements.

- **Maximum Building Setback (21A.34.090.D.2):** A maximum setback of 25 feet is required for at least thirty five percent (35%) of the building façade. To allow for a drive aisle for circulation of the drive-through lane, the building setback is proposed at 27 feet, which is the same distance of the current building. The building setback will not change with this redevelopment proposal.

If the drive aisle were removed from the front of the building, the building could be moved closer to State Street and allow space for increased landscaping and an outdoor dining area in front of the building, rather than to the side as proposed. These suggested features would improve the pedestrian appeal and promote the intent of the SSSC overlay district to encourage buildings closer to the street with parking on the sides or rear. The drive aisle could instead be located to the rear of the building or eliminated altogether. Locating the drive aisle in rear would allow for the same traffic movements anticipated by the propose plan but instead have the on site circulation occur behind the building. Eliminating the drive aisle would require customers driving south on State Street to continue to 1700 South and make a u-turn instead of turning left at Kensington.

Although neither of these circulation scenarios is the most desirable from the applicant's point of view, they do contribute to both pedestrian and vehicle oriented development more than the current or proposed traffic circulation. Not all commercial businesses can be located on a corner with four access points such as this business. Businesses located on a street with a raised median, and not on a corner lot, have to deal with the routing customers from the far side of median. This is a common traffic scenario arterial streets in the City and not peculiar to this site.

Another option would be to rotate the building and move it closer to State Street. By doing this, the main entrance would be oriented to State Street and a second entrance could be oriented to Kensington. The drive-through could be located at the rear of the building allowing vehicles to exit onto Kensington. This would allow the northern most access along State Street to be removed, which is a suggestion offered by a local citizen, Todd Draper, as a way to reduce turning conflicts at the intersection of State Street and Kensington. Rotating would also create more space east of the building to accommodate the drive-through, by-pass lane, and a drive aisle for circulation between parking lots. By demolishing the existing building, the site is essentially free to be developed in conformance with the current ordinance and design standards. Using the same building footprint is not a City requirement.

- **Parking Setback (21A.34.090.D.3):** The SSSC overlay district prohibits parking in the front and corner side yards. In this case, parking stalls are proposed to be located in the extreme northwest and southwest corners of the site, which conflict with the front yard along State Street. Proposed parking is also located within the corner side yard, which is along Kensington Avenue. Parking for the project will be located both north and south of the proposed building. The parking lot extends over three parcels, which should be combined as a condition of a project approval to avoid complications with off-site parking and perimeter parking lot landscaping requirements for each of the separate parcels.

Relief from the parking setback requirement in the front yard (State Street) is not necessary. One parking stall in the northwest corner and two stalls in the southwest corner could be converted to landscaped areas. As mentioned previously, the proposed number of parking stalls is more than required and eliminating up to ten to accommodate additional landscaping or rotating of the building is preferred. The parking proposed within the corner side yard (Kensington Ave) is realistically the only area available to park north of the proposed building and still provide sufficient backing clearance between the parked vehicles and those using the drive-through lane.

- **Number of parking stalls (Section 21A.44.060):** The proposed number of parking stalls exceeds the minimum number required. 34 stalls are proposed but, based on the building floor area of 3,862 square feet shown on site plan; the number of required parking stalls is 24 (6 per 1000 sq. ft. building area). For outdoor dining areas, no additional parking is required unless the seating capacity is being increased by more than five hundred (500) square feet. Parking for outdoor dining areas in excess of five hundred (500) square feet is required at a ratio of three (3) stalls per one thousand (1,000) square feet of outdoor dining area. This requirement may be waived if part of a Conditional Building and Site Design Review. The area of the proposed outdoor dining is approximately six hundred (600) square feet and accommodates three tables.

Staff supports waiving this additional parking requirement for outdoor dining based on the applicant's data that shows most of the customers to this site are drive-through customers. Drive-through customers tend to leave the site after receiving their food. Reducing the available parking also encourages mass transit and pedestrian traffic, which supports the goal of the SSSC overlay district.

- **Minimum First Floor Glass (21A.34.090.F.1):** The building facades facing State Street and Kensington are required to have a minimum of forty percent (40%) non-reflective glass. The proposed façade along Kensington Avenue, which is the drive-through side of the building, has approximately seven percent (7%) glass. The proposed façade along State Street has approximately eighteen percent (18%) glass.

Staff concedes that adding more windows along the drive-through lane façade lacks any real purpose. However, staff finds no hardship preventing 40% glass content on the west façade (State Street). In considering the aforementioned option of rotating the building and locating the drive-through lane on the east façade, the applicant could provide more glass content on the north (Kensington) and west (State Street) facades. This would be one more aspect whereby the project meets the intent of the SSSC Overlay and conforms to current ordinances.

- **Perimeter Parking Lot Landscaping (21A.48.070.C.2):** When parking is proposed within a required yard or within twenty feet (20') of a lot line, perimeter landscaping shall be provided. The landscaping shall be provided with landscape areas at least seven feet (7') wide measured from the back of the parking lot curb. The proposed parking lot extends into the front yard (State Street) at the extreme northwest and southwest corners; into the corner side yard (Kensington) along the north boundary, and; is within 20 feet of the rear (east) and interior side (south) property line. The proposed landscape areas along State Street, except the extreme southwest corner, satisfy the seven (7') foot minimum. The remainder of the perimeter parking lot landscaped areas range between three (3') and four (4') feet in width.

In this case, the excess parking is not needed. Between seven (7) and ten (10) stalls, depending on the size of the outdoor dining area, could be removed and the parking lot layout revised to increase the width of the perimeter landscaped areas to 7 feet in the extreme southwest corner and the two parking areas along the east property line. Given the small size of the parking area north of the building, and the existing landscaped park strip, staff supports the requested reduction of the perimeter landscape buffer along Kensington Avenue.

Comments

Public Comments

No comments were received from either of the community councils (People's Freeway, Liberty-Wells).

A representative from the facilities department of Salt Lake Community College suggested that pedestrian barriers (the existing chain link fence and concrete wall) located between the college campus and the McDonald's site be removed to allow for increased free flow of students. The applicant agreed with the suggestion and stated that they would remove the barriers along the east and south property lines and install a foot path leading from the existing sidewalk located on the college property to the restaurant. The foot path would have to cross the drive-through lanes but that was the only viable option. Students could also access through the southern parking lot.

Todd Draper, a citizen of the area, submitted comments related to traffic circulation; his comments are attached (See 'Exhibit E').

A subcommittee of the Planning Commission met twice to discuss the proposed redevelopment. The first meeting was held April 10 and the second meeting was on May 20, 2008 (See 'Exhibit F' for meeting notes). Commissioners Forbis and Muir attended the first meeting and generally supported the redevelopment as proposed subject to the applicant removing the drive aisle in front of the building and converting it to patio dining and landscaping; converting the striped areas within the parking lot to landscaped areas; adding more

glass to the north façade (Kensington Ave), and; providing pedestrian access from the building to the existing sidewalk on the Salt Lake Community College property to the east.

Commissioners Forbis, Chambless, and McDonough attended the second subcommittee meeting where the applicant focused on the issue of removing the drive aisle. The applicant pointed out that State Street has a raised median that complicates vehicle access and removing the drive aisle would prevent vehicles entering off Kensington from accessing the drive-through. Commissioners Forbis and Chambless requested data on number of drive-through customers versus walk-in customers. That data is provided with the attached applicant's project description (see 'Exhibit A'). Commissioner Chambless requested to see a lighting plan, which is included with the site drawings (see 'Exhibit B'). In summary, the Commissioners generally supported the redevelopment proposal.

City Department Comments (see 'Exhibit D')

Fire Department:

The department posed no objections or concerns.

Public Utilities:

The existing water and sewer lines servicing the restaurant can be used to service the new building provided they are in satisfactory condition. The existing lines must either be used, or taken out of service per the department's standards. A new 800 gallon outside grease interceptor with a sampling manhole will be required. All design and construction must conform to State, County, City and Public Utilities standards and ordinances.

Building Services and Licensing:

The division pointed out the issues of parking in the front and corner side yards, increased building setback, and minimum window area on the front building façade. These modifications are the subject of the Conditional Building and Site Design Review to be considered by the Planning Commissions.

City Engineering:

The department could find no aspect of the proposed project that involved development within the public way, however if the project does involve any work within the public way in the future, the work will require a Public Way Permit from the Engineering Division Office.

Transportation:

The department recommends consolidating the three existing parcels into one to resolve conflicts with the City's parking requirements. The applicant has made some revisions to the proposal during the application review process and the department requests revisions to the parking calculations. The applicant needs to include bicycle parking calculations in the overall parking calculations as specified in the ordinance. The areas within the parking lot that are indicated as landscaped areas must be outlined with a 6 inch curb. Final plan approval is subject to compliance with all parking geometrics and current city design standards.

Staff Analysis (Conditional Building and Site Design Review)

Conditional building and site design review shall be approved in conformance with the provisions of the following standards for design review found in chapter 21A.59.060 of the City's Zoning Ordinance.

- A. *The development shall be primarily oriented to the street, not an interior courtyard or parking lot.*

Analysis: State Street is considered the primary street for this development and is west of the site.

Parking lots are proposed for either side (north and south) of the restaurant building, with the southern parking lot being the larger. The proposed new restaurant building will be primarily oriented toward the south parking lot. The main building entrance faces south toward the parking lot. The outdoor dining

area is located on the south side of the building, on the same side as the main entrance, further orienting the development toward the southern parking lot. There is a building entrance facing State Street but it is not considered the main entrance. In considering the existing use of the site, the site development has always been oriented toward the parking lots, not the street. Orienting the new development toward the street would require significant reworking of the site and may not produce a desirable effect according to the applicant. However, by demolishing the existing building, the site is essentially free to be developed in a manner oriented to the street and in conformance with the current ordinance and design standards. Retaining the historical orientation is not a City requirement.

Finding: The proposed redevelopment does not satisfy this standard.

B. *The primary access shall be oriented to the pedestrian and mass transit.*

Analysis: The primary access and outdoor dining area are located on the building's south side. The pedestrian sidewalks and mass transit (bus) are both located along State Street to the west. Much of the anticipated foot traffic would come from Salt Lake Community College just east of the site. The students that come from Salt Lake Community College would likely access the site via a footpath to be installed near the southeast corner. The applicant has agreed to remove the existing concrete wall and chain link fence between the restaurant and Salt Lake Community College thereby allowing more efficient student pedestrian traffic to and from the college. The drive-through circulation places a drive aisle between State Street and the building, which causes conflict with pedestrians accessing from State Street. Removing the drive aisle, as discussed previously in this report, would remove this area of conflict from the front of the building. The proposed redevelopment has attempted to partially improve the pedestrian access between from the primary access to the adjacent college but overall has not oriented itself well to the broader, non-student pedestrian or mass transit user along State Street.

Finding: The proposed redevelopment does not satisfy this standard.

C. *The façade shall maintain detailing and glass in sufficient quantities to facilitate pedestrian interest and interaction.*

Analysis: The front façades of the proposed building will consist mainly of stucco and stone with customer entrances on the south (State Street) and north (Kensington Ave) sides of the building. The rear portion of the building will consist of mainly stucco. Windows are proposed for the western portion of the building and will be located on the south, east, and north facades. The facades fronting State Street and Kensington Avenue are required to contain a minimum of 40% glass. The façade on State Street contains approximately 18% glass and the façade on Kensington Avenue contains approximately 7% glass. The proposed amount of glass, coupled with the awnings and stone do facilitate some pedestrian interest but do not clearly satisfy the ordinance. The Kensington façade has minimal pedestrian interest on purpose, as it is the vehicle drive-through. The interior kitchen facilities are located along the Kensington wall and requiring more windows along this drive-through wall is not as crucial as along State Street. No hardship was found that would prevent 40% percent glass on the façade along State Street.

Finding: The proposed building facades partially meet this standard.

D. *Architectural detailing shall emphasize the pedestrian level of the building.*

Analysis: The proposed stone, stucco, window awnings, building relief, and roof top architectural features along the State Street facade all work to pique the interest of customers and help them recognize the restaurant. The architectural detailing on the Kensington façade is minimal, practical in purpose, and is geared toward the drive-through customer. This detailing works for both driving customers and pedestrian customers.

Finding: The proposed architectural detailing partially satisfies this standard.

E. *Parking lots shall be appropriately screened and landscaped to minimize their impact on the neighborhood.*

Analysis: The parking lots will be similar to the existing design, with some modifications to enhance the drive-through system and increase landscaping on the site. The perimeter parking lot landscaping along State Street and Kensington will be enhanced with additional shade trees and more shrubs in sufficient quantity to meet the landscaping requirements. The total amount of landscaped area will increase by approximately 1,800 square feet (3,944 sq ft to be increased to 5,736 sq ft). The redevelopment does not provide sufficient landscaping buffer width on the east lot line near the residentially zoned lots owned by the Salt Lake Community College or south property line. The Landscaping Ordinance stipulates a 7-foot perimeter parking lot buffer whenever the parking lot is within 20 feet of property lines. The landscape buffer proposed is approximately four feet. The existing site does not meet the ordinance but, with the redevelopment, will come closer to meeting the ordinance. The improvements and enhancements proposed by the applicant will increase the screening of the parking lot and increase the total amount of landscaping on the site.

Finding: The proposed redevelopment partially satisfies this standard.

F. *Parking lot lighting shall be shielded to eliminate excessive glare or light into adjacent neighborhoods.*

Analysis: A professionally prepared lighting plan was provided by the applicant. Staff review of this lighting plan indicates the lighting will be substantially contained within the property boundaries and will be forced downward and shielded by enclosed structures. Lighting will be adequate.

Finding: The proposed redevelopment satisfies this standard.

G. *Dumpsters and loading docks shall be appropriately screened or located within the structure.*

Analysis: The dumpster location will be in the southeast corner of the property. It will be enclosed with a structure of concrete wall and fencing. The area around the dumpster enclosure will be landscaped. The proposed enclosure and landscaping are sufficient. All product deliveries will be through a 'person' door located on the south side of the building facing the parking lot.

Finding: The proposed redevelopment satisfies this standard.

H. *Signage shall emphasize the pedestrian/mass transit orientation.*

Analysis: The existing pole sign along State Street will remain. The building façade along State Street will contain the McDonald's golden arch mounted on the wall, the word "McDonald's" mounted on the wall above the awnings, and a swooping yellow architectural feature above "McDonald's". This combination emphasizes both the vehicle customers and the pedestrian/mass transit customers.

Finding: The proposed redevelopment satisfies this standard.

I. *Any new development must comply with the intent of the purpose statement of the zoning district in which the project is located as well as adopted master plan policies and design guidelines governing the specific area of the proposed development.*

Analysis: "The purpose of the CC corridor commercial district is to provide an environment for efficient and attractive automobile oriented commercial development along arterial and major collector streets." "The purpose of the SSSC South State Street corridor overlay district is to acknowledge and reinforce the historical land development patterns along South State Street between 900 South and 2100 South," which have typically been locating buildings and landscaping in toward the front of the lot and parking on the sides or in the rear of the lot. The adopted master plan (Central Community) policies applicable to this project include commercial land use policy CLU-1.2: "Locate community level retail sales and services on appropriate arterials and do not encroach upon residential neighborhoods or generate community-wide parking and traffic issues."

The proposed restaurant is both a sit-down and drive-through restaurant. The building and parking lot design attempt to achieve a more efficient and attractive automobile oriented commercial development than currently exists at the site but does not serve the pedestrian or mass transit rider very well. The revised southern parking lot, traffic circulation, new building and increased landscaping all serve to improve the site and contribute to the purpose of the CC district.

The historical land development pattern along South State Street has been predominantly commercial strip type development with buildings in the front of the lot and parking on the sides or rear. Although this is oriented to the automobile, it also is oriented to the pedestrian and mass transit rider because the buildings and landscaped areas are closer to the street. The proposed redevelopment promotes the automobile at the expense of the pedestrian and mass transit rider and thereby does not fit with the purpose of the SSSC overlay district.

The location of this development on State Street, an arterial, satisfies the first part of the applicable Central Community master plan policy CLU-1.2, but not the second part which speaks to parking and traffic issues. By exceeding the required number of parking stalls, the proposed redevelopment continues its orientation to the automobile and missing the opportunity to more effectively involve the pedestrian and mass transit rider.

Finding: The proposed redevelopment satisfies the purpose of the CC zoning district, but not the purpose of the SSSC overlay district or the applicable policies of the Central Community master plan. Therefore, the proposed redevelopment only partially satisfies this standard.

Summary

The Conditional Building and Site Design Review process is intended to help ensure that redeveloped properties and newly developed properties are designed to encourage pedestrian access, circulation and orientation while acknowledging the need for transit and automobile access. The proposed redevelopment makes some improvements to the site in the areas of new buildings, increased landscaping and outdoor dining, but does not improve or encourage interaction with pedestrians or mass transit riders, which is a primary goal of the SSSC overlay district. Granted, the Conditional Building and Site Design Review was implemented as a process to modify development requirements, but the modifications requested by the applicant are those crucial to realizing the purposes of the SSSC overlay and Central Community master plan. Planning Staff recognizes the steps made by the applicant to improve the site but cannot support the project as proposed. Planning staff can support the project with the recommended conditions shown on the first page of this report.

ATTACHMENT 'A'
APPLICANT'S PROJECT DESCRIPTION



DOMINION

Engineering Associates, L.C.

February 11, 2008

Salt Lake City Planning
451 South State Street, Room 406
Salt Lake City, Utah 84111

RE: McDonald's Restaurant
1533 South State Street

Dear Planning:

I am filing for a Conditional Design Review for the McDonald's at 1533 South State Street. The restaurant is currently zoned CC. McDonald's Corporation would like to remodel the existing building. We have meet with the Development Review Team (DRT) on April 5, 2007 then again on September 11, 2007. It was decided that we could remodel less than 50% of the building. We have completed the site plan and architecture plan for less than 50%.

The problem that we have encountered is that due to the age of the building and trying to bring the building to current code is not cost effective. McDonald's Corporation would like to demolish the existing building and rebuild it with a building that meets code and McDonald's new building standards.

The new building will be built at the existing location. I have attached the site plan showing the location and revised landscape.

McDonald's Corporation respectfully requests that you consider this Conditional Design Review.

Sincerely,

DOMINION ENGINEERING ASSOCIATES, LC

E. Farley Eskelson, P.E.
Principal

attachment

copies: Mr. Steve Jenkins, McDonald's Corporation

McDonald's
SLC State Street
First Quarter 2008

Hour	Customers	D/T Customers	D/T % of Sales	D/T % of Trans	Avg Daily D/T GC's
6:00 AM	1,645	1,163	76.2%	70.7%	13
7:00 AM	4,115	2,841	71.3%	69.0%	31
8:00 AM	5,707	3,836	67.9%	67.2%	42
9:00 AM	5,856	3,603	63.8%	61.5%	40
10:00 AM	4,537	2,619	59.1%	57.7%	29
11:00 AM	4,625	2,520	56.2%	54.5%	28
Noon	7,118	4,098	58.3%	57.6%	45
1:00 PM	5,316	3,271	62.5%	61.5%	36
2:00 PM	4,176	2,571	63.1%	61.6%	28
3:00 PM	3,609	2,261	63.8%	62.6%	25
4:00 PM	3,172	1,979	64.9%	62.4%	22
5:00 PM	3,374	2,092	64.0%	62.0%	23
6:00 PM	3,123	1,937	66.3%	62.0%	21
7:00 PM	2,324	1,460	66.2%	62.8%	16
8:00 PM	2,142	1,302	63.7%	60.8%	14
9:00 PM	1,875	1,286	71.2%	68.6%	14
12:00 PM	1,172	883	75.1%	75.3%	10
Day Totals	63,914	39,746	63.9%	62.2%	437

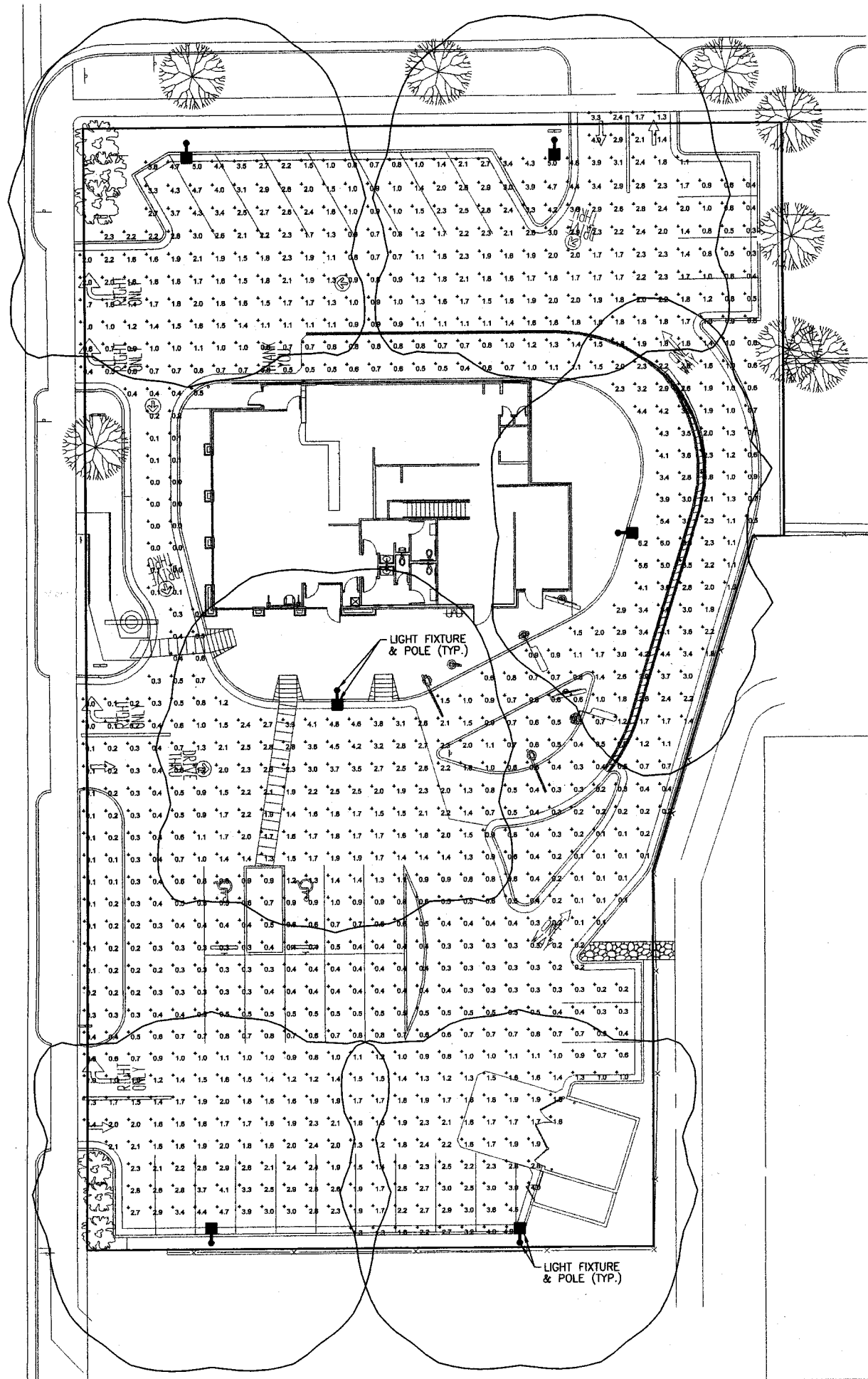
ATTACHMENT 'B'
SITE AND BUILDING DRAWINGS

KHAKI SCHEME



2008 STANDARD BUILDING GUIDELINES

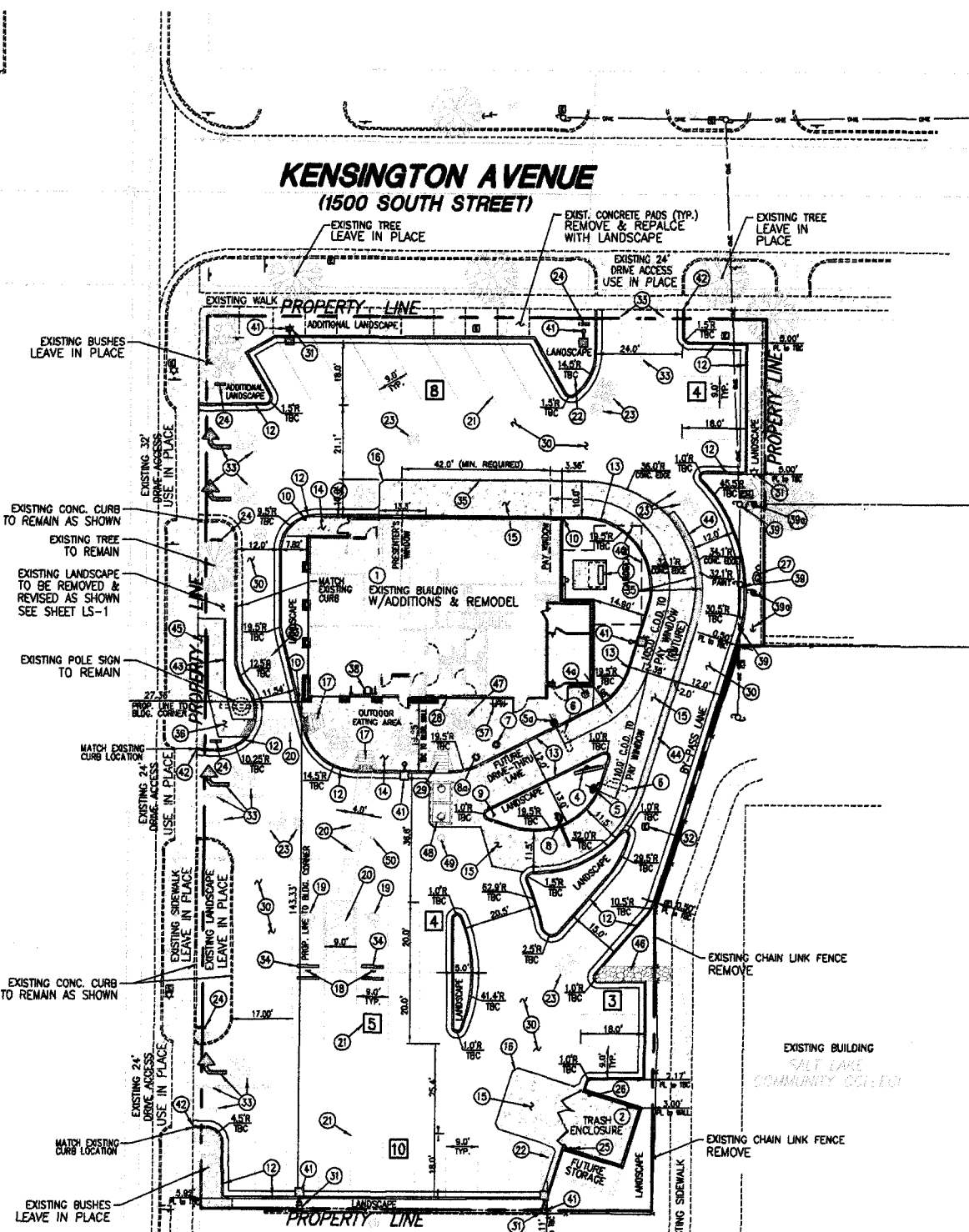




LIGHTING PLAN

STATE STREET

EXISTING RASSED MEDIAN



**KENSINGTON AVENUE
 (1500 SOUTH STREET)**

NOTE: DRIVE-THRU LANE (CLOSEST TO BUILDING)

CONTRACTOR TO INSTALL ALL UNDERGROUND WIRING FOOTINGS, DETECTOR LOOPS, ETC. DURING INITIAL SITE IMPROVEMENT CONSTRUCTION FOR FUTURE USE (AS NEEDED) FOR 2nd MAIN MENU BOARD, 2nd "GATEWAY" SIGN, 2nd CUSTOMER ORDER DISPLAY, AND "PRE-SELL" MENU BOARD

SITE PLAN IMPROVEMENTS

SITE PLAN NOTES SCHEDULE

- 1 NEW BUILDING ADDITIONS & REMODEL - SEE ARCHITECTURAL PLANS
- 2 NEW TRASH ENCLOSURE, SEE DETAILS, SHEET SD-4
- 3 NEW ELECTRICAL TRANSFORMER & CONC. PAD (APPROX. LOCATION)
- 4 RELOCATED EXIST. MENU BOARD, SEE DETAILS, SHTS. SD-2 & SD-3
- 4a NEW FOUNDATION AND NECESSARY WIRING FOR FUTURE MENU BOARD, SEE DETAILS, SHEETS SD-2 & SD-3
- 5 NEW CUSTOMER ORDER DISPLAY "OOD" BOX, SEE DETAILS, SHEET SD-3
- 5a NEW FOUNDATION AND NECESSARY WIRING FOR FUTURE C.O.D., SEE DETAILS, SHEETS SD-2 & SD-3
- 6 NEW LOOP DETECTOR, SEE DETAILS, SHEET SD-3
- 7 NEW FOUNDATION AND NECESSARY WIRING FOR FUTURE PRE-SELL MENU BOARD, SEE DETAILS, SHEET SD-3
- 8 NEW "GATEWAY"/HEIGHT DETECTOR SIGN, SEE DETAILS, SHEETS SD-2
- 8a NEW FOUNDATION AND NECESSARY WIRING FOR FUTURE "GATEWAY"/HEIGHT DETECTOR SIGN, SEE DETAILS, SHEET SD-2
- 9 NEW BOLLARD SIGN "THIS LANE ONLY"
- 10 RELOCATED EXIST. PIPE BOLLARD, SEE FOUNDATION DETAIL, SHEET SD-2
- 11 RELOCATED EXIST. GUARDRAIL, INSTALLED BY G.C.
- 12 NEW COMMERCIAL 24" HIGH BACK CURB AND GUTTER, SEE SHEET SD-5
- 13 NEW 6" HIGH CONCRETE CURB WALL
- 14 NEW 4" THICK CONCRETE WALK/SLAB (WIDTH VARIES, SEE PLAN)
- 15 NEW MINIMUM 6" CONCRETE SLAB, REINF. WITH FIBERMESH, OR PER CITY OF SALT LAKE REQUIREMENTS
- 16 2' CHAMFER AT CORNERS OF CONCRETE (TYPICAL)
- 17 NEW HANDICAP RAMP PER A.D.A. STANDARDS
- 18 NEW HANDICAP SIGN, TOP OF SIGN @ 5' ABOVE SIDEWALK (TYP.)
- 19 NEW HANDICAP SYMBOL, PAINTED PER ADA STANDARDS
- 20 NEW HANDICAP ACCESS STRIPING, PAINTED PER ADA STANDARDS
- 21 NEW 4" WIDE PARKING LINES PAINTED WHITE (TYPICAL)
- 22 NEW 4" WIDE @ 45° ANGLE @ 2' O.C. NO PARKING LINES PAINTED (WHITE)
- 23 NEW PAINTED GRAPHICS PER McDONALD'S STANDARDS & SPECIFICATIONS
- 24 DIRECTIONAL SIGN (EXISTING, CHANGED & NEW) SEE SIGNAGE DRAWING
- 25 NEW HOSE BIBB WITH BACKFLOW PREVENTOR
- 26 NEW 1" PVC CONDUIT FOR FUTURE POWER (IF NEEDED)
- 27 EXISTING EXTERNAL GAS METER - USE IN PLACE
- 28 1/2" EXPANSION JOINT W/BACKER ROD & SEALANT (TYPICAL BETWEEN BLDG. FOUNDATION AND BLDG. WALK)
- 29 NEW CONCRETE DELIVERY RAMP
- 30 NEW ASPHALT PAVING (TYP.), SEE SECTION, SHEET SD-5
- 31 EXISTING OUTDOOR LOT LIGHT, REMOVE & REPLACE WITH NEW
- 32 EXISTING ELECTRIC BOX, RELOCATE TO LANDSCAPE AREA
- 33 NEW PAINTED GRAPHICS PER SALT LAKE CITY STANDARDS & SPECIFICATIONS
- 34 EXISTING RELOCATED CONCRETE WHEEL-STOP
- 35 NEW 6" YELLOW STRIPE PAINTED (PMS 123), PER McDONALD'S STDS.
- 36 NEW 4" THICK CONCRETE HANDICAP ACCESS SIDEWALK SEE SHEET SP-4 SLOPE & GRADES
- 37 NEW BICYCLE RACK PER SALT LAKE CITY CITY REQUIREMENTS
- 38 EXISTING RELOCATED "RED BOX" KIOSK VIDEO OUTLET (LOCATION TO BE DETERMINED BY OWNER/OPERATOR)
- 39 EXISTING POWER POLES, GUY ANCHORS - RELOCATE AS SHOWN
- 39a RELOCATED EXISTING POWER POLES, GUY ANCHOR (APPROXIMATE LOCATION PER ROCKY MOUNTAIN POWER)
- 40 PROPOSED POWER EASEMENT
- 41 NEW LIGHT POLE/FIXTURE, SEE SHEET LTG-1
- 42 TAPER NEW CURB DOWN TO MATCH SIDEWALK GRADE (TYPICAL)
- 43 ADA APPROVED HANDRAILS FOR HANDICAP ACCESS SIDEWALK
- 44 24" WIDE CONCRETE WATERWAY, SEE DETAIL, SHEET SD-5
- 45 HANDICAP ACCESS SIGN PER ADA STANDARDS & SPECIFICATIONS
- 46 ROCK PATHWAY FOR PEDESTRIAN ACCESS
- 47 OUTDOOR PICNIC TABLES PER OWNER/OPERATOR
- 48 1200 GAL. GREASE INTERCEPTOR, SEE DETAIL SH. SD-5
- 49 SAMPLING MANHOLE PER SALT LAKE CITY PUBLIC UTILITIES STANDARDS & SPECIFICATIONS
- 50 SEWER LATERAL CLEANOUT PER SALT LAKE CITY PUBLIC UTILITIES STANDARDS & SPECIFICATIONS

BUILDING EXPANSION

CITY APPROVED UP TO 49% OF GROSS EXISTING BUILDING
 EXISTING GROSS AREA = 2,856 SQ. FT.
 49% OF 2,856 SQ. FT. = 1,399 SQ. FT.
 PROPOSED BUILDING EXPANSION AREA = 997± SQ. FT.

LANDSCAPE AREA

CITY APPROVED LANDSCAPE AREA NOT BE LESS THAN EXISTING LANDSCAPE AREA AFTER SITE IMPROVEMENTS
 TOTAL EXISTING LANDSCAPE AREA = 3,944 SQ. FT.
 TOTAL PROPOSED LANDSCAPE AREA = 5,736 SQ. FT.

PROJECT DATA CHART - McDONALD'S (EXISTING SITE)
 -QUANTITIES FOR McDONALD'S COST ESTIMATES ONLY-

SALT LAKE CITY ZONING	CC, SSSC
TOTAL GROSS ACREAGE	0.822 Ac. (35,795 Sq. Ft.)
TOTAL BUILDING AREA	3,862± Sq. Ft.*
NUMBER OF PARKING SPACES	34
TOTAL ASPHALT SURFACE	20,013 SQ. FT.*
TOTAL 6" THICK CONCRETE SLAB	3,697 SQ. FT.*
TOTAL 4" THICK CONCRETE SLAB	1,806 SQ. FT.*
TOTAL 24" CONCRETE CURB & GUTTER	935 LIN. FT.*
TOTAL 6" HIGH CONCRETE CURB WALL	270 LIN. FT.*
TOTAL EXISTING CURB TO REMAIN	105 LIN. FT.
TOTAL LANDSCAPING (BY McDONALD'S)	5,736 Sq. Ft.
TOTAL PERCENTAGE OF LANDSCAPING	16.0%

*QUANTITIES NEEDED TO COMPLETE CONSTRUCTION ON TOTAL SITE

UTILITY NOTE

THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM ABOVE GROUND SURVEY TESTS AND/OR EXISTING UTILITY DRAWINGS AND RECORDS. THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT POSITION INDICATED, ALTHOUGH HE DOES STATE THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM THE INFORMATION MADE AVAILABLE TO HIM. BEFORE DIGGING, CONTACT EACH UTILITY ENTITY SO THAT THEY MAY MARK THEIR LOCATIONS ON THE GROUND.

UTILITY CONTACTS		
WATER & STORM	SALT LAKE CITY PUBLIC UTILITIES	801-463-8729
SEWER	SALT LAKE CITY PUBLIC UTILITIES	801-463-8729
ELECTRIC POWER	UTAH POWER & LIGHT	888-221-7070
GAS	QUESTAR GAS	800-323-5517
TELEPHONE	QUEST	800-803-8000



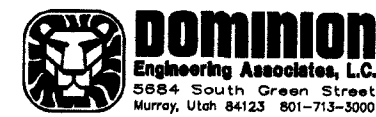
GENERAL NOTES		BY	ISSUE REF.
1. ALL WORK PERFORMED WITHIN THE SALT LAKE CITY RIGHT-OF-WAY SHALL BE IN ACCORDANCE WITH THEIR STANDARDS AND SPECIFICATIONS. 2. BASES, ANCHOR BOLTS, CONDUIT, AND WIRING FOR ALL SIGNS ARE BY THE GENERAL CONTRACTOR. McDONALD'S SIGN AND BASE ARE BY THE SIGN CONTRACTOR. FLAGPOLE ANCHOR BOLTS ARE BY LANDSCAPERS. ALL SIGNAGE SHALL COMPLY WITH THE REQUIREMENTS OF THE SALT LAKE CITY CODE. 3. GENERAL CONTRACTOR TO PROVIDE CONCRETE BRICK, CONDUIT & WIRE AS REQUIRED FOR LIGHT FIXTURES. McDONALD'S TO PROVIDE ONLY ANCHOR BOLTS, POLES AND LOT LIGHT FIXTURES. REMANDER BY GENERAL CONTRACTOR. 4. ALL RAIN ARE TO THE TOP BACK OF CURB (TBC) UNLESS OTHERWISE NOTED. VERIFY IN FIELD (I.F.) ALL DIMENSIONS. RESOLVE ANY DISCREPANCIES WITH McDONALD'S PROJECT MANAGER PRIOR TO COMMENCING WORK. 5. PROPOSED UTILITIES ARE SHOWN IN SCHEMATIC ONLY. EXACT LOCATIONS SHALL BE DETERMINED TO ALLOW FOR THE MOST ECONOMICAL INSTALLATION. 6. THE CONTRACTOR SHALL COORDINATE WITH ALL UTILITY COMPANIES TO DETERMINE EXACT POINT OF SERVICE CONNECTION AT EXISTING UTILITY. REFER TO THE BUILDING ELECTRICAL AND PLUMBING DRAWINGS FOR UTILITY SERVICE ENTRANCE LOCATIONS, SIZES AND CIRCUITING. GENERAL CONTRACTOR RESPONSIBLE TO OBTAIN ALL REQUIREMENTS OF UTILITY COMPANIES AND INCLUDE IN BASE BID ALL ASSOCIATED COSTS. 7. ALL ELEVATIONS SHOWN ARE IN REFERENCE TO THE BENCHMARK AND MUST BE VERIFIED BY THE GENERAL CONTRACTOR AT GEOLOGIC/SPAC. FINISH FLOOR ELEVATION IS AS NOTED ON GRADING AND DRAINAGE PLAN. 8. GENERAL CONTRACTOR IS RESPONSIBLE FOR COMPLIANCE WITH ALL REQUIREMENTS OF THE "AMERICAN'S WITH DISABILITIES ACT" (ADA). 9. ALL 4" CONCRETE VERTICAL CURB SHALL BE POURED MONOLITHICALLY WITH CONCRETE PAVEMENT. FINISH WALK AND CURB ELEVATIONS SHALL BE 4" ABOVE FINISH PAVEMENT UNLESS NOTED OTHERWISE. 10. GENERAL CONTRACTOR TO PLACE CONCRETE CONSTRUCTION JOINTS AS NOTED WITH MAXIMUM AREA OF 400 S.F. (CAREY FELT SHALL NOT BE USED). 11. GENERAL CONTRACTOR TO PROVIDE SHOULDER JOINTS IN CONC. PAVEMENT IN ACCORDANCE WITH A.C.I. STANDARDS. JOINTS SHALL BE PLACED SUCH THAT THE MAXIMUM SIZE OF CONCRETE SLAB IS 120 S.F. AND THE RATIO OF THE SIDES IS NO MORE THAN 2:1. 12. COORDINATE ALL DRIVE-UP STRIPING AND MARKINGS WITH McDONALD'S PROJECT MANAGER. 13. CONTRACTOR TO FIELD VERIFY LOCATIONS OF EXISTING MANHOLES AND OTHER UTILITIES BEFORE STARTING OR BUILDING ANY WORK. 14. ALL CONSTRUCTION SHALL COMPLY WITH CITY OF SALT LAKE WATER COMPANY CLIMATE WATER AND/OR SANITARY SEWER SYSTEM EXTENSION AGREEMENT WITH SPECIFICATIONS, DOCUMENTATION, TYPICAL DETAILS AND FORMS. 15. LOT LIGHTING CONC. FOOTINGS TO CONFORM WITH THE SOILS REPORT RECOMMENDATIONS FOR THIS PARTICULAR SITE, OR AS DIRECTED BY McDONALD'S. 16. ALL LANDSCAPE AREAS SHALL BE ROUGH GRADED TO 6" BELOW TOP OF ALL BULBS AND CURBS. FINISH GRADING, LANDSCAPING, AND SPRINKLER SYSTEMS ARE BY THE OWNER/OPERATOR.		5/29/08 REV 1	ADDITIONAL LANDSCAPE AREAS PROVIDED FROM COLLEGE PER CITY REQUEST PEDESTRIAN ACCESS FROM COLLEGE PER CITY REQUEST
PAVING SPECIFICATION (WITHIN McDONALD'S CORPORATION PROPERTY)			
ASPHALT: COMPACTION: 2" A.C. (2 LIFTS) 95% MARSHALL 7" BASE COURSE 95% PROCTOR OIL CONTENT: 4.5% - 6.0% STABILITY: 100 LBS. MIN. GRADATION EXTRACTION: 55% MAX. SHALL PASS U.S. STD. #4 SIEVE.			
CONCRETE: COMPACTION: 2" CONCRETE 7" BASE COURSE 95% PROCTOR			
NOTES: 1. McDONALD'S OWNER RESERVES THE RIGHT TO REQUEST A COMPACTION TEST AND/OR A CORE SAMPLE IF TESTS PRIOR TO CONSTRUCTION DO NOT MEET THE SPECIFICATION. TESTS WILL BE AT THE EXPENSE OF McDONALD'S. OTHERWISE, E.G. WILL BE CHANGED. 2. SEE SOILS REPORT BY ?			
PARKING INFORMATION (EXISTING)			
SPACES	2 HC SPACES	9'-0" x 18'-0" @ 90°	
51	8 SPACES	9'-0" x 18'-0" @ 90°	
	41 SPACES	9'-0" x 18'-0" @ 90°	
PARKING INFORMATION (PROPOSED)			
SPACES	2 HC SPACES	9'-0" x 20'-0" @ 90°	
34	8 SPACES	9'-0" x 18'-0" @ 90°	
	17 SPACES	9'-0" x 18'-0" @ 90°	
	7 SPACES	9'-0" x 20'-0" @ 90°	
SALT LAKE CITY SPEC. REQUIREMENT: 6 per 1,000 sq. ft. GROSS BUILDING AREA = 2870 sq. ft. (18 REQUIRED)			
SURVEY INFORMATION			
PREPARED BY: DOMINION ENGINEERING 5684 South Green St. Murray, Utah 84123 (801) 713-3000			
DATE: July, 2005			
LEGEND			
EXISTING CURB	FACE OF CURB	FC	
EXISTING BUILDING	PAINT STRIPE	PS	
PROPOSED CURB	TOP BACK OF CURB	TBC	
PROPOSED CONC. EDGE	EDGE OF CONCRETE	EC	
PROPOSED CONCRETE	RADIUS	R	
	PROPOSED PARKING STALL #	X	
PLAN SCALE: 1" = 20'			
STREET ADDRESS 1533 SOUTH STATE STREET			
CITY		STATE	
SALT LAKE CITY		UTAH	
COUNTY		COUNTY	
SALT LAKE		SALT LAKE	
REGIONAL DWG. NO	CORPORATE DWG. NO.		
STATUS		DATE	BY
PRELIMINARY			
PLAN CHECKED			
AS-BUILT			
SP-2			



2375 E. CAMELBACK ROAD, 5th Floor, PHOENIX, AZ 85016

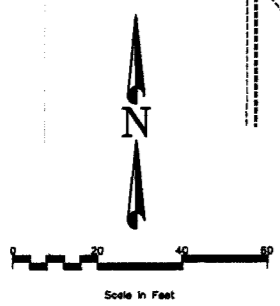
DATE	DATE	DATE	DATE

REGIONAL DWG. NO	CORPORATE DWG. NO.

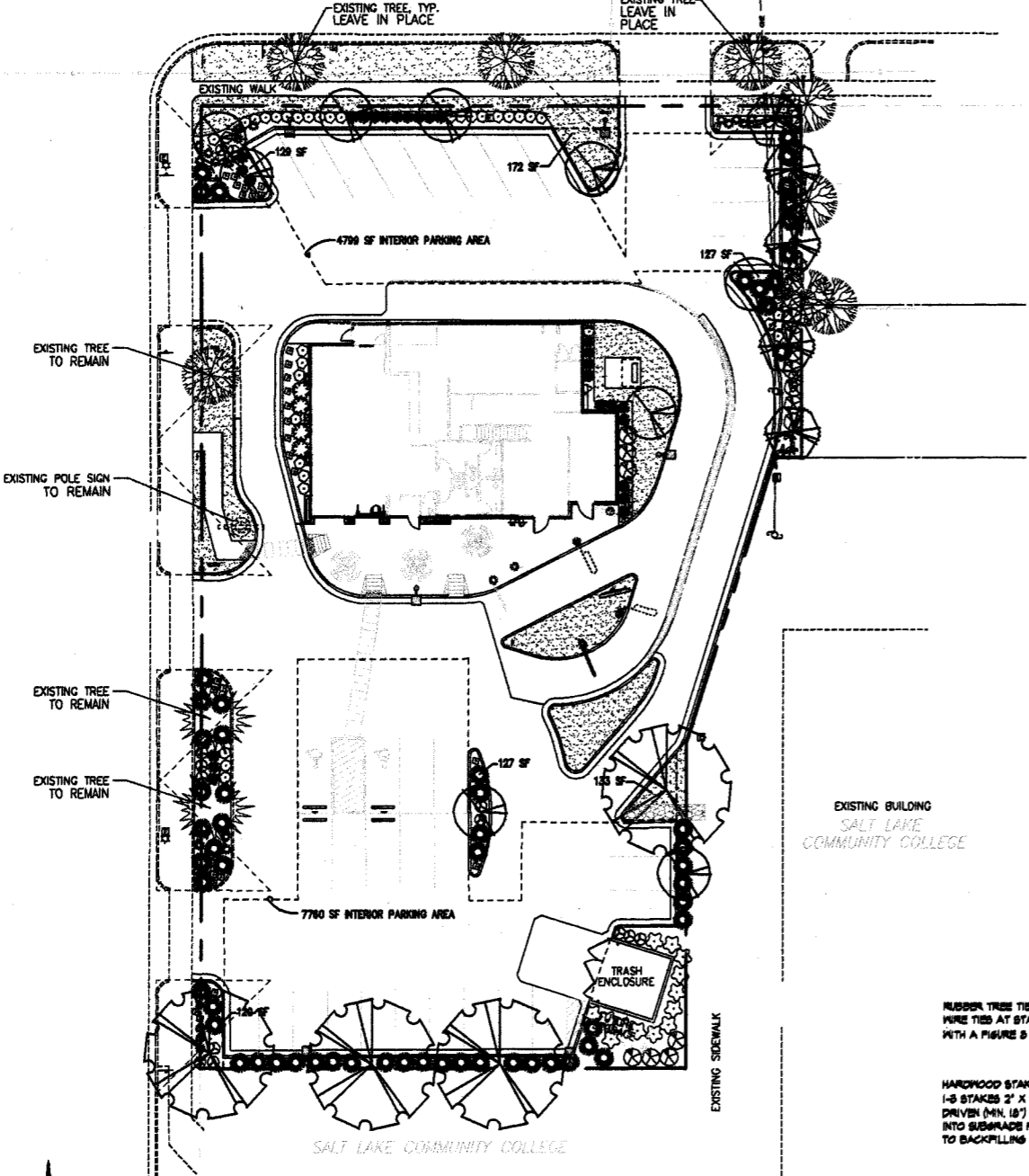


STATE STREET
EXISTING RAISED MEDIAN

EXISTING RAISED MEDIAN



KENSINGTON AVENUE
(1500 SOUTH STREET)



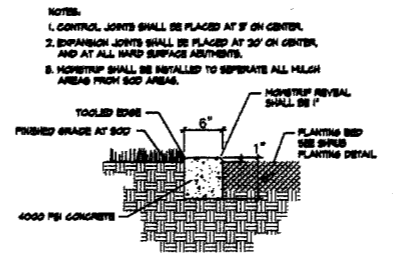
LANDSCAPE PLAN

PLANT LIST

Trees		
Quantity	Scientific Name Common Name	Planting Size
5	Acer glabre 'Flame' Amur Maple	1 1/2" - Caliper
4	Gleditsia triacanthos 'Skyline' Skyline Honeylocust	1 1/2" - Caliper
9	Cheniclear Pear Pyrus calleryana 'Cheniclear'	1 1/2" - Caliper
Shrubs		
Quantity	Scientific Name Common Name	Planting Size
39	Spiraea bumalda 'Goldmound' Burnside spiraea	5-Gal
21	Euonymus alatus comp. 'Dwarf' Dwarf Burning Bush	5-Gal
60	Juniperus sibirica 'Broadmoor' Broadmoor Juniper	5-Gal
28	Buxus semp. 'Green Velvet' Green Velvet Boxwood	5-Gal
4	Juniperus 'Blue Star' Blue Star Juniper	5-Gal
13	Euonymus japonica 'Silver King' Silver King Euonymus	5-Gal
13	Potentilla fruticosa 'Tangerine' Tangerine Conqueror	5-Gal
9	Buxus semp. 'Green Min. Pyr.' Green Min. Pyramidal Boxwood	5-Gal
19	Caryopteris don. 'Parsons Select' Parsons Blue Mist Spirea	5-Gal
Perennials / Ground Cover		
Quantity	Scientific Name Common Name	Planting Size
39	Hemerocallis stala de Oro Daylily	1-Gal
2,800 SF	3 Blend (Min.) Bluegrass Sod	SOD

PLANTING NOTES:

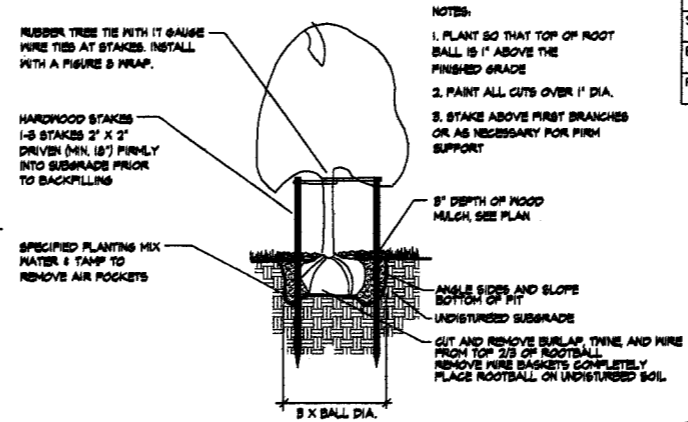
- LANDSCAPE BIDS MUST BE IN A DETAILED UNIT PRICE FORMAT SHOWING COST BREAKDOWN OF ALL LANDSCAPE ELEMENTS. ANY ELEMENT MAY BE ADDED OR DELETED BY THE LANDSCAPE ARCHITECT OR OWNER. CONTRACTOR COMPENSATION WILL BE ADJUSTED ACCORDING TO THE UNIT PRICE BIDS. ANY SUBSTITUTION MUST BE BY LANDSCAPE ARCHITECT APPROVAL. LANDSCAPE ARCHITECT WITHOUT JUSTIFICATION MAY DISALLOW BIDS AND SELECT CONTRACTOR REGARDLESS OF BID FIGURES. THE SELECTED CONTRACTOR MUST CONTACT THE OWNER'S CONSTRUCTION REPRESENTATIVE BEFORE BEGINNING CONSTRUCTION TO SCHEDULE A PRE CONSTRUCTION MEETING. CONTRACTOR OR SUBCONTRACTORS PARTICIPATION IN CONSTRUCTION OF THIS PLAN MUST BE LICENSED BY THE STATE OF UTAH FOR THE TYPE OF WORK BEING PERFORMED.
- CODES, LAWS, REGULATIONS, AND PERMITS BY FEDERAL, STATE, COUNTY AND CITY AGENCIES FOR DESIGN CONCEPT, MATERIALS AND WORKMANSHIP MUST BE RESEARCHED AND SATISFIED BY THE CONTRACTOR. REPORT ANY PROBLEMS OR REQUIREMENTS TO THE LANDSCAPE ARCHITECT. THE CONTRACTOR MUST VERIFY THE REGULATIONS FOR AND SECURE ANY PERMITS BEFORE BEGINNING CONSTRUCTION. THE COST FOR THE PERMIT FEES MAY BE SUBMITTED TO THE OWNER FOR REIMBURSEMENT. CALL BLUE STAKES AND REFER TO DRAINAGE AND CIVIL PLANS BEFORE ANY TRENCHING OR EXCAVATION.
- CONSTRUCTION SAFETY AND CLEANUP MUST MEET OSHA STANDARDS AT ALL TIMES. ALL CONTRACTORS MUST HAVE ADEQUATE LIABILITY, PERSONNEL INJURY AND PROPERTY DAMAGE INSURANCE. CLEAN UP MUST BE PERFORMED DAILY, AND ALL HARDSCAPE ELEMENTS MUST BE WASHED FREE OF DIRT AND MUD ON FINAL CLEAN UP. CONSTRUCTION MUST OCCUR IN A TIMELY MANNER.
- LANDSCAPE PLANS AND DETAIL DRAWINGS ARE SCHEMATIC ONLY. DISCREPANCIES MAY EXIST, INCLUDING BUT NOT LIMITED TO BUILDING LOCATION, PROPERTY LINES, ANY DIMENSIONS SPECIFIED OR IMPLIED. THE CONTRACTOR WILL BE REQUIRED TO ADJUST PLANS AS NECESSARY TO RETAIN CONCEPT INTEGRITY. CONTACT LANDSCAPE ARCHITECT IF DISCREPANCIES EXIST.
- PLANT MATERIAL EXCAVATION. CALL BLUE STAKE AND MAKE REFERENCE TO DRAINAGE AND CIVIL PLANS BEFORE EXCAVATION FOR PLANT MATERIAL. ALL HOLES MUST ALLOW FOR A MINIMUM OF SIX (6) INCHES OF SPECIFIED PLANTING MIX BACKFILL MATERIAL ON ALL SIDES OF ROOT BALL FOR SHRUBS, AND 3X BALL DIAMETER FOR TREES.
- PLANT MATERIAL BACKFILL MUST BE A WELL MIXED COMBINATION OF 1/3 NATIVE SOIL, 1/3 TOPSOIL, AND 1/3 ORGANIC COMPOSTED MATERIAL. DEEP WATER ALL PLANT MATERIAL IMMEDIATELY AFTER PLANTING. ADD BACKFILL MATERIAL TO DEPRESSIONS AS NECESSARY.
- SOIL AMENDING SHALL INCLUDE COMPOSTED ORGANIC MATERIAL TO BE ADDED AT A RATE OF FOUR CUBIC YARDS PER 1000 SQUARE FEET. TILL INTO THE SOIL TO A DEPTH OF 6". ALL SOD AND SHRUB AREAS SHALL HAVE THE SOIL AMENDED.
- TOP SOIL MUST BE A PREMIUM QUALITY DARK SANDY LOAM, FREE OF ROCKS, CLODS, ROOTS, AND PLANT MATTER. THE TOPSOIL WILL BE EVENLY SPREAD AND SMOOTH GRADED ON A CAREFULLY PREPARED SUBGRADE TO A DEPTH OF THREE INCHES (3") IN ALL FLOWERING ANNUAL BEDS, CONTAINERIZED GROUNDCOVER AREAS, AND AREAS TO BE SOODED.
- SOD MUST BE PREMIUM QUALITY, ULTRA GREEN, EVENLY CUT, ESTABLISHED, HEALTHY, WEED AND DISEASE FREE, AND FROM AN APPROVED SOURCE. SOD MUST BE DELIVERED AND LAID IMMEDIATELY AFTER CUTTING. SOD MUST BE LAID WITH NO GAPS BETWEEN PIECES ON A CAREFULLY PREPARED TOPSOIL LAYER. THE LAID SOD MUST BE IMMEDIATELY WATERED AFTER INSTALLATION. ANY BURNED AREAS WILL REQUIRE REPLACEMENT. ADJUST SPRINKLER SYSTEM TO ASSURE HEALTHY GREEN SURVIVAL OF THE SOD WITHOUT WATER WASTE.
- SHREDDED WOOD MULCH WILL BE REQUIRED IN ALL LANDSCAPE BEDS FOR SHRUBS, PERENNIALS, AND ANNUALS. THE MULCH SHALL BE APPLIED OVER DOWNTYP PROS WEED BARRIER. MULCH SHALL BE EVENLY SPREAD ON A CAREFULLY PREPARED GRADE TO A MINIMUM DEPTH OF THREE (3) INCHES. THE TOP OF ALL AREAS OF MULCH SHALL BE AT THE GRADE OF THE ADJACENT CURB, WALK, OR EDGE OF PAVEMENT.
- FERTILIZER FOR SOD AREAS SHALL BE PELLETED, N-P-K AS APPROVED BY LANDSCAPE ARCHITECT FOR SEASONAL ADJUSTMENT. USE 20 LBS PER 5,000 SQUARE FEET OR AS PER MANUFACTURER'S SPECIFICATIONS. SPREAD EVENLY ON A CAREFULLY PREPARED TOPSOIL LAYER JUST PRIOR TO LAYING SOD.
- TREE STAKING AND GUYING SHALL BE ON AN AS NEEDED BASIS ONLY. THE CONTRACTOR SHALL DETERMINE STAKING NEEDS DEPENDENT ON SITE SOIL CONDITIONS. IT IS THE CONTRACTORS RESPONSIBILITY TO REMOVE TREE GUYING AND STAKING IN A TIMELY MANNER ONCE STAKED TREES HAVE TAKEN ROOT. NO STAKING SHALL REMAIN BEYOND A REASONABLE TIME FOR ROOT PENETRATION AND STABILIZATION.
- TREE WRAPPING MAY BE USED TO PROTECT YOUNG TREES FROM WINTER DAMAGE. TREE WRAPS SHALL ONLY BE INSTALLED IN THE FALL. IF THE CONTRACTOR INSTALLS WRAPS FOR TREE PROTECTION IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO PROMPTLY REMOVE WRAPS THE FOLLOWING SPRING.
- LANDSCAPE MAINTENANCE MUST BE PERFORMED BY THE LANDSCAPE CONTRACTOR DURING ESTABLISHMENT (30 DAYS AFTER FINAL ACCEPTANCE OF ENTIRE PROJECT). RESPONSIBILITIES INCLUDE WEED CONTROL AND MOWING. NOTIFY OWNER AND CONSTRUCTION REPRESENTATIVE WHEN ESTABLISHMENT PERIOD HAS ENDED TO INSURE ONGOING MAINTENANCE. THE OWNER IS RESPONSIBLE FOR LANDSCAPE MAINTENANCE AND UPKEEP ONCE ESTABLISHMENT PERIOD HAS EXPIRED.
- ALL PLANT MATERIAL AND LANDSCAPE ELEMENTS WILL BE GUARANTEED FOR ONE YEAR AFTER FINAL ACCEPTANCE. ANY ITEMS THAT ARE NOT FIRST CLASS PREMIUM QUALITY WILL BE REPLACED BY THE CONTRACTOR AT NO COST TO THE OWNER. ANY PLANT MATERIAL THAT IS NOT PREMIUM QUALITY OR APPEARS STRESSED IN ANY WAY DURING THE GUARANTEE PERIOD MAY REQUIRE REPLACEMENT. THE CONTRACTOR MUST SCHEDULE A PRE AND POST GUARANTEE MEETING WITH THE OWNER'S REPRESENTATIVE FOR INSPECTION. FAILURE TO DO SO WILL MEAN THE OFFICIAL GUARANTEE PERIOD HAS NOT BEEN ACTIVATED OR DE-ACTIVATED.
- IT IS THE CONTRACTORS RESPONSIBILITY TO VERIFY ALL QUANTITIES LISTED ON THE PLANS AND THE AVAILABILITY OF ALL PLANT MATERIALS AND THEIR SPECIFIED SIZES PRIOR TO SUBMITTING A BID. THE CONTRACTOR MUST NOTIFY THE LANDSCAPE ARCHITECT PRIOR TO SUBMITTING A BID IF THE CONTRACTOR DETERMINES A QUANTITY DEFICIENCY OR AVAILABILITY PROBLEM WITH SPECIFIED MATERIAL.
- SPECIFICATIONS FOR LANDSCAPE AND IRRIGATION CONSTRUCTION SHALL BE THE 2007 APWA "MANUAL OF STANDARD SPECIFICATIONS".
- ALL LANDSCAPE MATERIAL SHALL BE FULLY IRRIGATED BY AN AUTOMATIC IRRIGATION SYSTEM (DESIGN BUILT). IRRIGATION DESIGN SHALL BE APPROVED BY THE OWNER AND LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.



CONCRETE MOWSTRIP
N.T.S.



SHRUB PLANTING
N.T.S.



TREE PLANTING / STAKING
N.T.S.

Landscape Calculations Summary

	Required	Provided
Parking lot area		
12,559 sf		
Interior parking lot landscape area	628 SF	817 SF
(12,559 sf x .05=628 sf)		
Interior parking lot trees	6	6
(628 sf / 120=5.2 trees)		
State Street Perimeter Parking Lot Trees / Shrubs	3 Tr./49 Sh.	3 Tr./49 Sh.
(146' / 50) 2.9 trees, (146' / 3) 49 shrubs		(2 Tr. Existing)
Kensington Perimeter Parking Lot Trees / Shrubs	3 Tr./41 Sh.	3 Tr./41 Sh.
(124' / 50) 2.5 trees, (124' / 3) 41 shrubs		
Southern Perimeter Parking Lot Trees / Shrubs	2 Tr./15 Sh.	2 Tr./17 Sh.
(92' / 50) 1.8 trees, (92' / 6) 15 shrubs		
South East Perimeter Parking Lot Trees / Shrubs	1 Tr./5 Sh.	1 Tr./6 Sh.
(28' / 50) .6 trees, (28' / 6) 5 shrubs		
East Perimeter Residential Landscape Buffer	3 Trees	4 Trees
(92' / 30) 3.1 trees, solid shrub screen	Shrub Screen	Shrub Screen
Percent of drought tolerant Trees and Shrubs	80%	100%
(18 of 18 Trees; 204 of 204 Shrubs)		



PLAN SCALE: 1" = 20'

STREET ADDRESS
1533 SOUTH STATE STREET

CITY STATE
SALT LAKE CITY UTAH

COUNTY
SALT LAKE

REGIONAL DWG. NO. CORPORATE DWG. NO.



McDonald's

THESE PLANS AND SPECIFICATIONS ARE THE PROPERTY OF MCDONALD'S CORPORATION AND SHALL NOT BE REPRODUCED WITHOUT THEIR WRITTEN PERMISSION.

OFFICE REGION ADDRESS 2375 E. CAMELBACK ROAD, 5th Floor, PHOENIX, AZ 85016

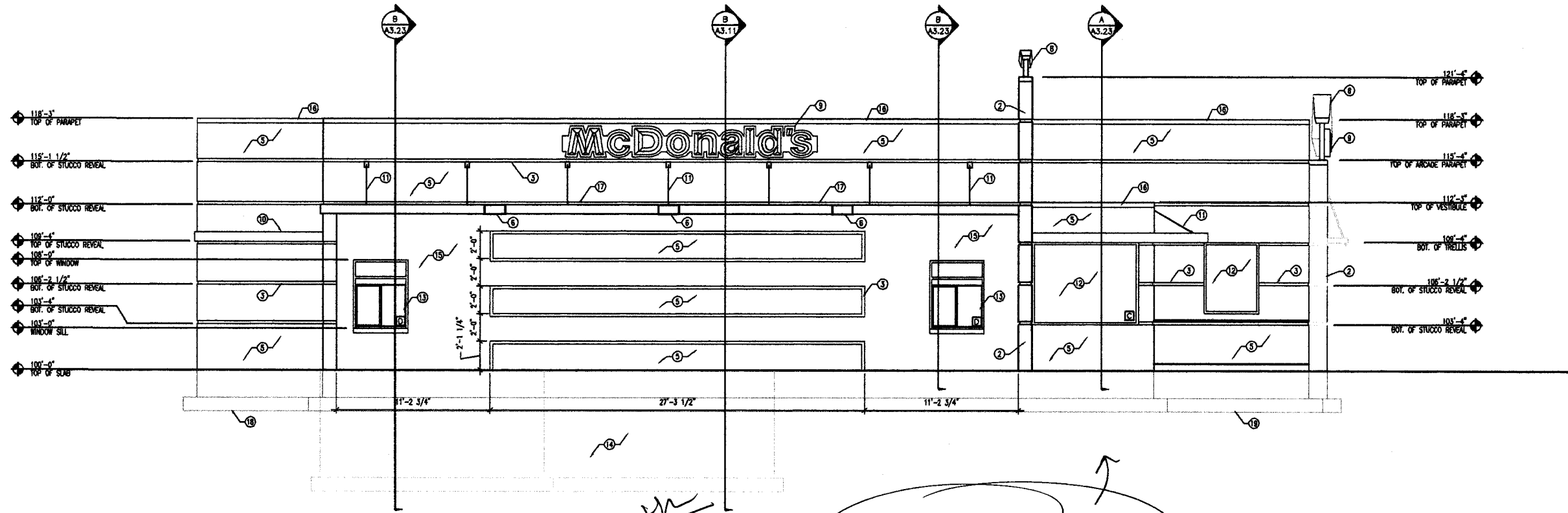
PLAN APPROVALS		DATE
SIGNATURE (2 REQUIRED)		
REGIONAL MGR.		
CONTR. MGR.		
OPERATIONS MGR.		
REAL ESTATE DEPT.		
CO-SIGN SIGNATURES		
CONTRACTOR		
OWNER		

STATUS	DATE	BY
PRELIMINARY		
PLAN CHECKED		
AS-BUILT		

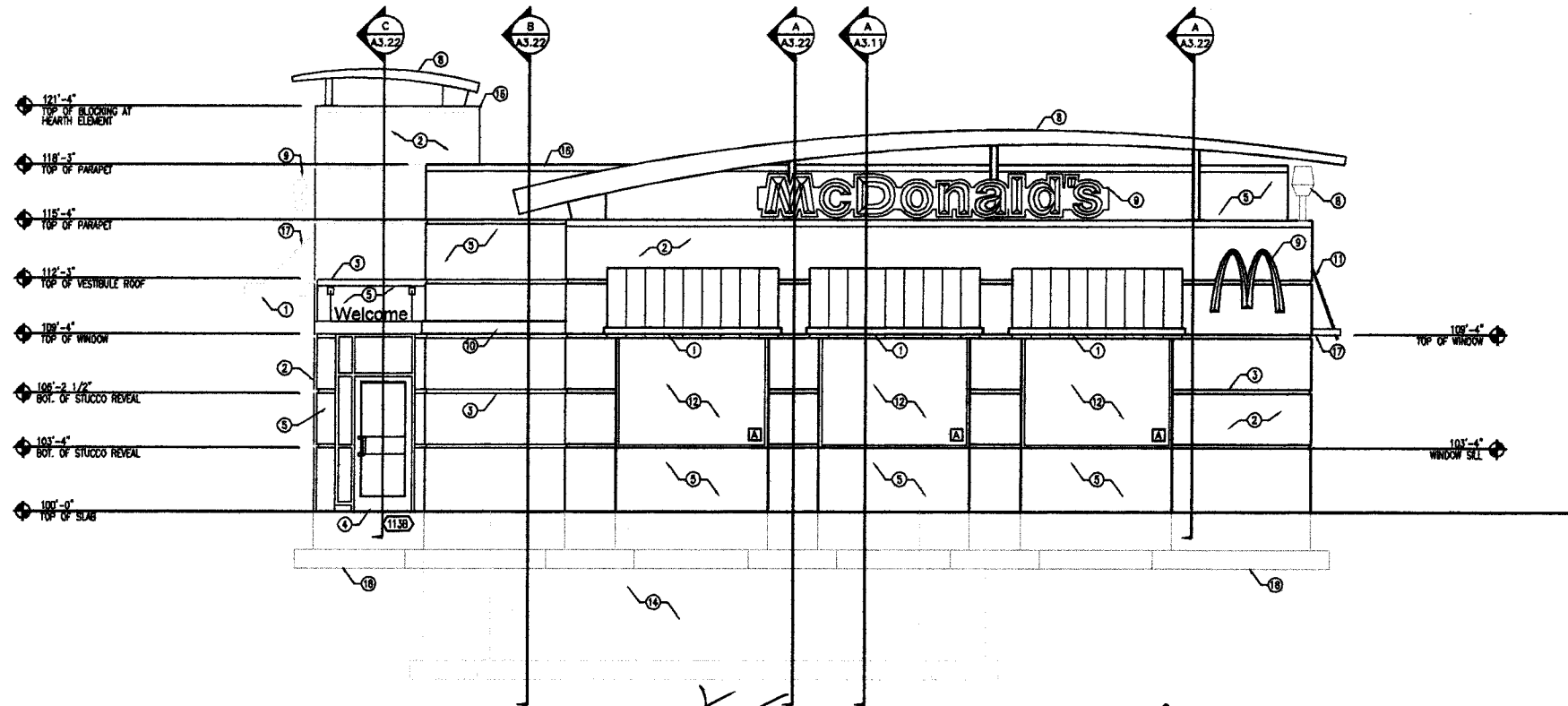
LS-1

KEYED NOTES

1. OWNER PROVIDED CONTRACTOR INSTALLED METAL AWNING - UNDER SEPARATE PERMIT - COLOR TO BE ALTERNATING STRIPES TO MATCH PANTONE 123C AND 109C
2. EXTERIOR INSULATION FINISH SYSTEM COLOR: BENJAMIN MOORE SNOW WHITE 2122-70 OR EQUAL
3. EPS REVEAL - TYPICAL
4. STOREFRONT DOOR ASSEMBLY - SEE DOOR SCHEDULE
5. EXTERIOR INSULATION FINISH SYSTEM COLOR: BENJAMIN MOORE ALEXANDRIA BEIGE HC-77 OR EQUAL
6. LIGHT FIXTURE - WALL SCONCE - SEE ELECTRICAL
7. PIPE BOLLARD - PAINTED YELLOW
8. ROOF ELEMENT BY OTHERS
9. McDONALD'S SIGNAGE BY OTHERS - UNDER SEPARATE PERMIT - SEE SIGNAGE SUPPORT NOTE ON THIS SHEET
10. METAL TRELLIS SYSTEM BY OTHERS
11. TRELLIS TIE BACK
12. EXTERIOR WINDOW ASSEMBLY - SEE ASSEMBLY NOTES ON SHEET ALSO
13. DRIVE THRU WINDOW BY READY ACCESS - CONFIRM MODEL, OPTIONS, AND SIZE WITH McDONALD'S PROJECT MANAGER. OPTIONS INCLUDE: TRANSOM, AIR CURTAIN, FLYFAN / TRANSOM AND 432 SQUARE INCHES MAXIMUM SERVICE OPENING
14. EXISTING FOOTING & FOUNDATION - PROTECT DURING CONSTRUCTION
15. EXTERIOR INSULATION FINISH SYSTEM COLOR: BENJAMIN MOORE MONROE BEISOLE HC-28 OR EQUAL
16. PREFINISHED METAL PARAPET CAP
17. OWNER PROVIDED CONTRACTOR INSTALLED METAL CANOPY UNDER SEPARATE PERMIT
18. CONCRETE FOOTING AND FOUNDATION - SEE STRUCTURAL



North
A ELEVATION
 SCALE: 1/4" = 1'-0"
 Kensington Ave.



West
B ELEVATION
 SCALE: 1/4" = 1'-0"
 (State Street)



evans + associates architecture
 11574 South State Street, Suite 103b, Draper, Utah 84020
 phone 801-553-8272 fax 801-553-8273



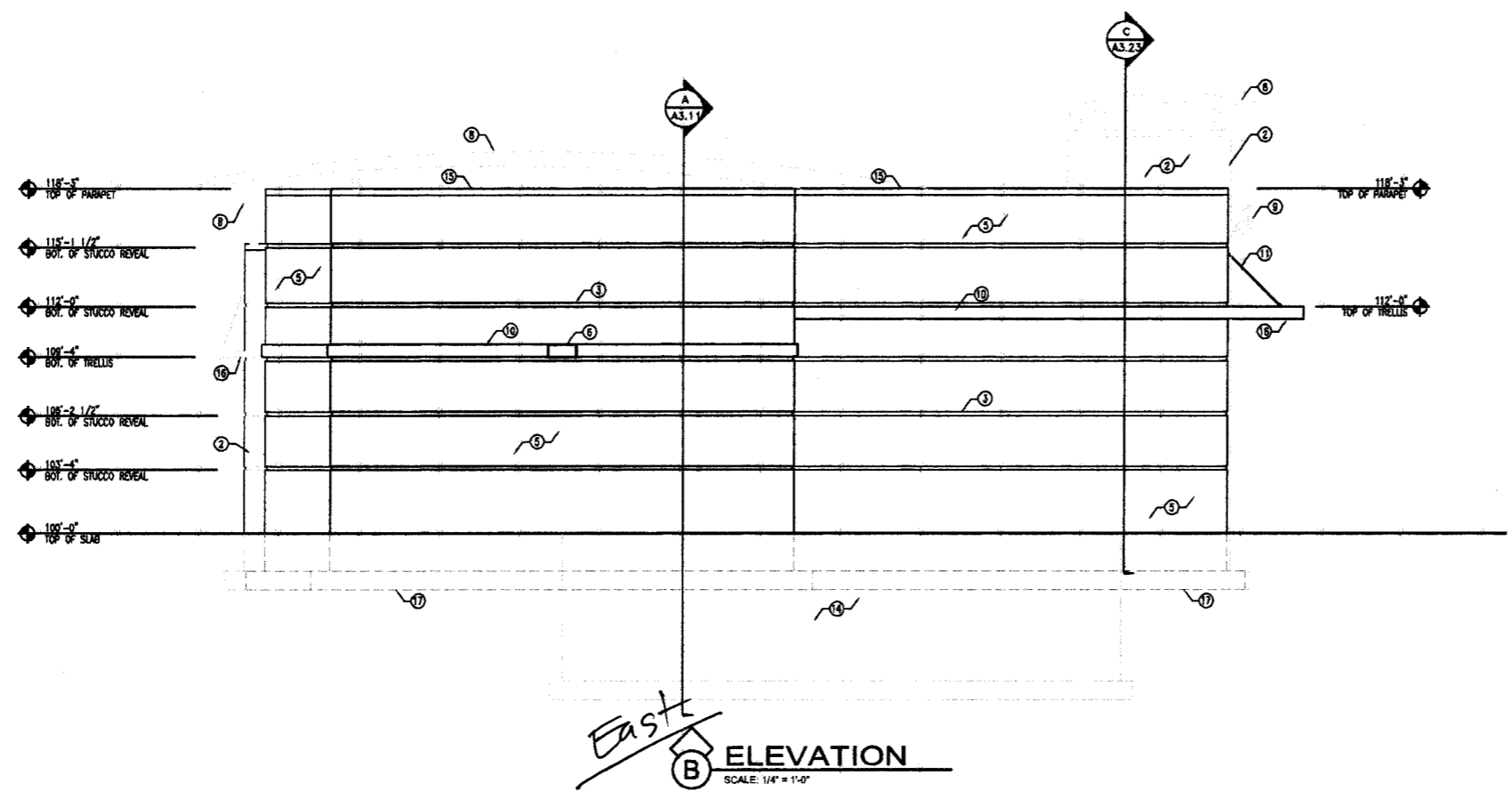
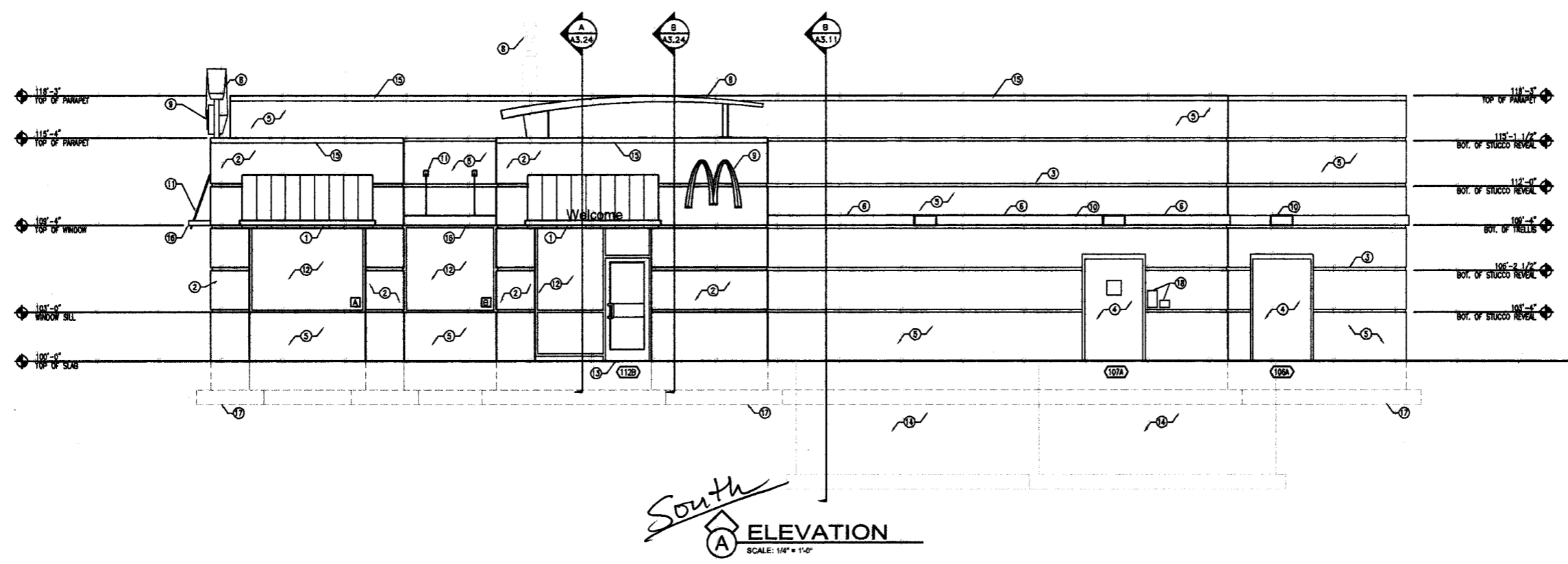
April 1, 2008 Project No. 07-58

Revisions

ELEVATIONS
A2.2

KEYED NOTES

1. OWNER PROVIDED CONTRACTOR INSTALLED METAL FINISHING - UNDER SEPARATE PERMIT - COLOR TO BE ALTERNATING STRIPES TO MATCH PANTONE 123C AND 109C
2. EXTERIOR INSULATION FINISH SYSTEM COLOR: BENJAMIN MOORE SNOW WHITE 2122-70 OR EQUAL
3. EPS REVEAL - TYPICAL
4. HOLLOW METAL DOOR - PAINT TO MATCH COLOR OF SURROUNDING MATERIAL
5. EXTERIOR INSULATION FINISH SYSTEM COLOR: BENJAMIN MOORE ALEXANDRIA BEIGE HC-77 OR EQUAL
6. LIGHT FIXTURE - BALL SCONCE - SEE ELECTRICAL
7. PIPE BOLLARD - PAINTED YELLOW
8. ROOF ELEMENT BY OTHERS
9. MCDONALD'S SIGNAGE BY OTHERS - UNDER SEPARATE PERMIT - SEE SIGNAGE SUPPORT NOTE ON THIS SHEET
10. METAL TRELIS SYSTEM BY OTHERS
11. TRELIS TIE BACK
12. EXTERIOR WINDOW ASSEMBLY - SEE ASSEMBLY NOTES ON SHEET A3.0
13. STOREFRONT DOOR ASSEMBLY - SEE DOOR SCHEDULE
14. EXISTING FOOTING & FOUNDATION - PROTECT DURING CONSTRUCTION
15. PREFINISHED METAL PARAPET CAP
16. OWNER PROVIDED CONTRACTOR INSTALLED METAL CANOPY - UNDER SEPARATE PERMIT
17. CONCRETE FOOTING AND FOUNDATION - SEE STRUCTURAL
18. CO2 FILL BOX - SEE PLUMBING
19. BULK OIL FILL BOX - CONFIRM USE WITH MCDONALD'S



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April 1, 2008 Project No. 07-68

Revisions

ELEVATIONS

A2.1

ATTACHMENT 'C'
SITE PHOTOGRAPHS:



1



2



3



4



5



6



7

ATTACHMENT 'D'

DEPARTMENT COMMENTS

Engineering

Stewart, Casey

From: Smith, Craig
Sent: Tuesday, March 18, 2008 10:57 AM
To: Stewart, Casey
Cc: Walsh, Barry; Weiler, Scott; Ott, George
Subject: petition #430-08-01

Good morning Casey-

I have reviewed petition #430-08-01 involving a conditional design review. All of the issues are on private property. If there are no issues within the public way on city property, then Engineering has no involvement.

Sincerely,
Craig

3/18/2008

Stewart, Casey

From: Garcia, Peggy
Sent: Friday, March 21, 2008 12:37 PM
To: Stewart, Casey
Subject: 430-08-01 McDonald's 1533 South State Street
Categories: Program/Policy

Casey,

Public Utilities has reviewed the above mentioned petition and offer the following comments;

According to our records there is an existing one-inch water meter and four-inch sanitary sewer lateral servicing this property. The existing meter can remain to provide culinary and irrigation services. The existing sewer lateral can also remain provided it is in satisfactory condition. All exiting utilities must be used or be killed per SLC Public Utilities standards. A new minimum 800 gallon outside grease interceptor with a sampling manhole will need to be installed to service this facility.

All design and construction must conform to State, County, City and Public Utilities standards and ordinances. Design and construction must conform to Salt Lake City Public Utilities General Notes.

Fire Department approval will be required prior to Public Utilities approval. Fire flow requirements, hydrant spacing and access issues will need to be resolved with the fire department.

Jason Brown, PE

Development Review Engineer
Salt Lake City Public Utilities
1530 South West Temple
Salt Lake City, UT 84115
(801) 483-6729
(801) 483-6855 fax
jason.brown@slcgov.com

Stewart, Casey

From: Walkingshaw, Nole
Sent: Thursday, March 20, 2008 4:13 PM
To: Stewart, Casey
Cc: Butcher, Larry
Subject: 430-08-01 Mc Donald's Conditional Design Review
Categories: Program/Policy

Casey,

Building services has the following comments:

- Plans show the building as an existing building with a proposed addition. Notes from DRT state that the proposal is to demolish existing structure and build a new structure. This should be clarified on the plans.
- The standards for an increased set back and minimum front façade glazing are established in the ordinance and may be reviewed by the Planning Commission
- The standards for modified parking requirements in the corner side and front yards are established in the ordinance and may be reviewed by the Planning Director.

Thanks,

Nole

Nole Walkingshaw
Salt Lake City Planning and Zoning
Senior Planner/Zoning Administration
801-535-7128

Fire Dept.

Stewart, Casey

From: Itchon, Edward → Fire Dept.
Sent: Friday, March 14, 2008 4:34 PM
To: Stewart, Casey
Cc: Butcher, Larry; McCarty, Gary; Montanez, Karleen
Subject: 430-08-01 Mc Donalds rebuilt project

No Issues

Transportation**Stewart, Casey**

From: Walsh, Barry
Sent: Thursday, March 20, 2008 2:38 PM
To: Stewart, Casey
Cc: Young, Kevin; Smith, Craig; Itchon, Edward; Garcia, Peggy; Butcher, Larry
Subject: Pet 430-08-01 McDonald's
Categories: Program/Policy

March 20, 2008

Casey Stewart, Planning

Re: Conditional design review pet 430-08-01 for McDonald's rebuild project at 1533 South State St.

The division of transportation review comments and recommendations as follows:

The plat indicates 3 different parcels. A cross easement or combining the parcels is required for the parking and circulations.

The parking calculations indicates that there were 51 parking spaces and now only 32 are being provided. Revise the parking calculations as need to be presented for the entire site (building sf and out door dinning over 500 sf) ? 22 to 25 stalls required. The ADA stalls (2) are shown with one being van accessible and the bike rack is noted but not calculated in the 5% requirement.

The parking lot revision along the north property line should have a 6" curb and landscaping rather than the paint marking shown also the area to the east of the building needs to be labeled as landscaping.

Sincerely,

Barry Walsh

Cc Kevin Young, P.E.
Craig Smith, Engineering
Ted Itchon, Fire
Peggy Garcia, Public Utilities
Larry Butcher, Permits
File

ATTACHMENT 'E'
PUBLIC COMMENTS

Stewart, Casey

From: Todd Draper [TDraper@slco.org]
Sent: Wednesday, May 28, 2008 3:46 PM
To: Stewart, Casey
Subject: McDonald's Concerns and Issues
Attachments: McDonald's Concerns and Issues.doc

Casey,

I have attached a word document with a written and graphical representation of my concerns regarding the site. I really don't know how much leeway the City has in forcing the issues, but it is worth bringing up at a Planning Commission meeting whether or not McDonald's is really trying to do what is best for the neighborhood (traffic wise). Thanks.

Todd A. Draper
tdraper@slco.org

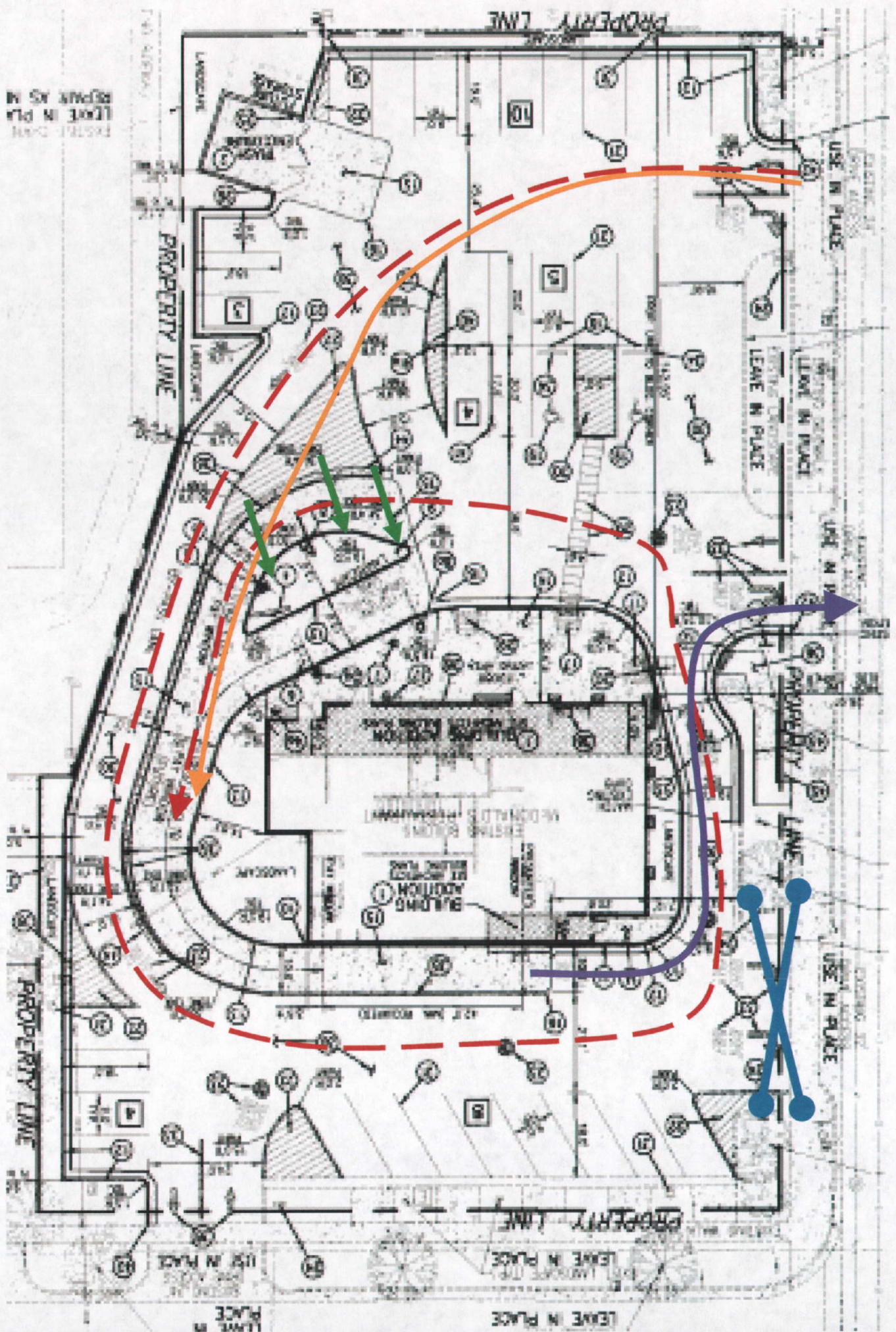
There is a lot of automobile congestion at this site and I don't think that this plan addresses it appropriately.

1. The red line shows the proper traffic flow (as proposed) for someone entering in the first driveway entrance coming from the South to the North, it is ridiculous. The proper flow to reduce congestion should be to along the yellow line. The proposed landscaping should be moved to increase the size of the center island (green arrows).

2. There is a substantial amount of traffic (due to SLCC) at the intersection of 1500 south and State Street with Southbound cars on State Street turning left and conflicting with vehicles leaving the McDonalds at the Most Northern State Street exit as it is so close to the corner. The exit should be eliminated (Blue X) and additional landscaping installed. The Drive though traffic and pass through traffic could then simply exit along the path indicated in purple.

The main idea should be to eliminate as much traffic as possible in front of the front door to reduce auto/pedestrian conflicts, especially where they are proposing adding outdoor seating.

3. As the second drive up order lane is only "proposed" and may never go in, what landscaping will they add in its place until such time they decide to install it? In my opinion they should just install the two drive up system now as there is a long line of traffic in the mornings (before class) and at noon. (This is also why I would recommend that the first drive through lane be accessed from the South drive as there it provides more room on the site to stack automobiles away from the front door)



EXISTING PLAN
LEFT IN PLACE
RIGHT AS IS

PROPERTY LINE

PROPERTY LINE

PROPERTY LINE

USE IN PLACE

LEAVE IN PLACE

PROPERTY LINE

USE IN PLACE

PROPERTY LINE

LEAVE IN PLACE

USE IN PLACE

LEAVE IN PLACE

LEAVE IN PLACE

ATTACHMENT 'F'

PC Subcommittee notes

Planning Commission Subcommittee

April 10, 2008

Attendees:

Planning Commission: Robert Forbis, Prescott Muir.

Planning Division Staff: Casey Stewart

Background and Project Location: Site is currently occupied by a McDonald's restaurant and located at 1533 South State Street.

Presentation in summary including changes to the project: This property is abutting the Salt Lake City Community College on State Street. The maximum setback is 25' (feet) and parking is not allowed on the front or corner side yards, however, the applicant is proposing to do so. The applicant also proposed a new building, using the same foot print, because structurally the current building does not meet code, and also has a basement which is irrelevant for McDonald's. The applicant will also be requesting that the 40 percent glass requirement be waived for the facades along State Street and Kensington, because the windows on the north side of the building will be removed to keep the kitchen enclosed.

Staff/Subcommittee recommendation(s), comments and concerns:

The sub-committee members were supportive of modifying the front setback requirements to allow rebuilding in the same location; allow parking in the front and corner side yard as proposed, and reducing the amount of required glass content on the north and west façades in the following manner and subject to the following conditions:

- Remove the drive aisle between the building and State Street and replace it with patio dining and landscaping
- Convert the painted/striped areas in all areas of the parking lot with curb and landscaping (total of 5 locations: 2 north, 1 northeast, 2 south)
- Add more grass/landscaped area north of the building (convert 2 or 3 parking stalls to landscaping)
- Wrap windows around from west face to north face in a similar fashion and quantity as shown on the south/southwest corner of building.
- Provide pedestrian access (sidewalk preferred) from McDonald's building to existing sidewalk located on SLCC property southeast of McDonald's building.

Planning Commission Subcommittee

May 20, 2008

Attendees:

Planning Commission: Robert Forbis, Tim Chambless, and Peggy McDonough.

Planning Division Staff: Casey Stewart

Applicant: Farley Eskelson (Engineer for McDonald's), Doug Wheelwright (Consultant)

Background and Project Location: McDonald's 1533 South State Street. This project was heard by a Planning Commission Subcommittee originally on April 10, 2008 by Commissioners Robert Forbis and Prescott Muir, and an open house has been held for public comment.

Presentation in summary including changes to the project: This property is abutting the Salt Lake City Community College on State Street. The maximum setback is 25' (feet) and parking is not allowed on the front or corner side yards; however, the applicant is proposing to do so, and will be increasing the landscaping on the property from 3,944 square feet to 5,470 square feet to help mitigate this request. The applicant also proposed a new building, using the same foot print, because structurally the current building does not meet code, and also has a basement which is irrelevant for McDonald's. The applicant will also be requiring that the 40 percent glass requirement be waived, because the windows on the north side of the building will be removed to keep the kitchen enclosed, but the windows on the North West corner will stay.

The applicant has spoken with the community college and it has been requested that the fencing running along the property line should be removed for easier student access to the restaurant from the college.

Because of the median down the center of State Street the drive-through is not accessible to southbound traffic; the applicant is proposing a second access from Kensington Avenue to access the drive-through queue.

Mr. Wheelwright commented that the driveway width was 12' (feet) wide because of the double lane drive-through access, which could not be made smaller, but the applicant was willing to mitigate this with the additional landscaping.

Staff/Subcommittee recommendation(s), comments and concerns:

Commissioner McDonough inquired if the connection to the drive-through was being mitigated by internal circulation on both sides of the building. She inquired if there would be available outside dining, which would help make the area more walkable, and McDonald's a main destination.

Commissioner Chambless requested a specific traffic study for the McDonald's location.

Commissioner Forbis requested the times that this location was most busy.

Mr. Eskelson stated that McDonald's kept track of both of these requests in detail and he would be happy to provide that. He noted that most of the business at this location was 60 percent drive-through and 40 percent walk-ins.

Commissioner Chambless also wanted to see outdoor dining.

Mr. Eskelson stated that there was a concrete pad where at least three tables could be put in to accommodate this.

Mr. Wheelwright noted that there was a lot of green space on the community college property right next door and with the fence being removed; this is where a lot of people might choose to go and eat.

Conclusion:

- **Commissioners agreed that this project was ready to be put on a Planning Commission agenda and the plans reflected the best utilization of the property considering space constraints.**
- **Commissioners agreed they would like tables added for outside dining.**
- **Commissioner Chambless requested an in-depth presentation of the lighting that will be used on the premises, which would allow for optimum safety. Mr. Eskelson stated he would submit that to Mr. Stewart before the meeting. Commissioner Forbis suggested additional lighting on the east side of the property, abutting the community college, for night students.**